



City of Bayswater Character Protection Areas Policy



**Maylands North
Mount Lawley
Bayswater**

City of Bayswater

Character Protection Areas Policy

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Introduction

One important way by which the local community identifies itself is its cultural heritage, mainly represented by the built environment. The local community wishes to ensure that development in certain areas is sympathetic to the local character.

The character of these residential areas can be simply expressed as their streetscape appeal; this includes front fences and gardens, the scale and form of existing buildings, roofscapes, heritage places and trees that combine to form the street's character.

The City designated Character Protection Areas (CPA) and established specific guidelines for these areas ensure new development is sympathetic to the local character of the respective areas.

Development applications are to be mindful of the Character Statement for each of the three character areas and these character qualities should be reflected in individual design proposals within these areas.

1.0 OBJECTIVE

- *To ensure that new development in Character Protection Areas is sympathetic with the character, rhythm, scale and visual amenity of existing residential streetscapes in the three Character Protection Areas.*

In this policy 'streetscape' is interpreted as incorporating all of the visual elements which inform the perception of a street; these include the road, front fences and gardens, scale and form of existing buildings, roofscapes, trees and open spaces, that form a street's character.

Reference to Related Planning Provisions

Scheme Provisions

City of Bayswater Town Planning Scheme No 24 (TPS 24)

Clause 8.5.5.1 of TPS 24 establishes *that "The purpose of a Character Protection Area is to ensure that development in these areas, as shown on the Scheme Map, is sympathetic to the prevailing character and streetscape of the locality"*

Clause 8.5.5.2 requires that development within a Character Protection Area shall demonstrate a high degree of compliance with Clause 8.5.5.1 (above), *"In the case of redevelopment in excess of a single house"... "shall reflect those architectural elements and features of existing buildings which contribute significantly to the streetscape or character of an area"*, and shall comply with any relevant Policies adopted by the Council.

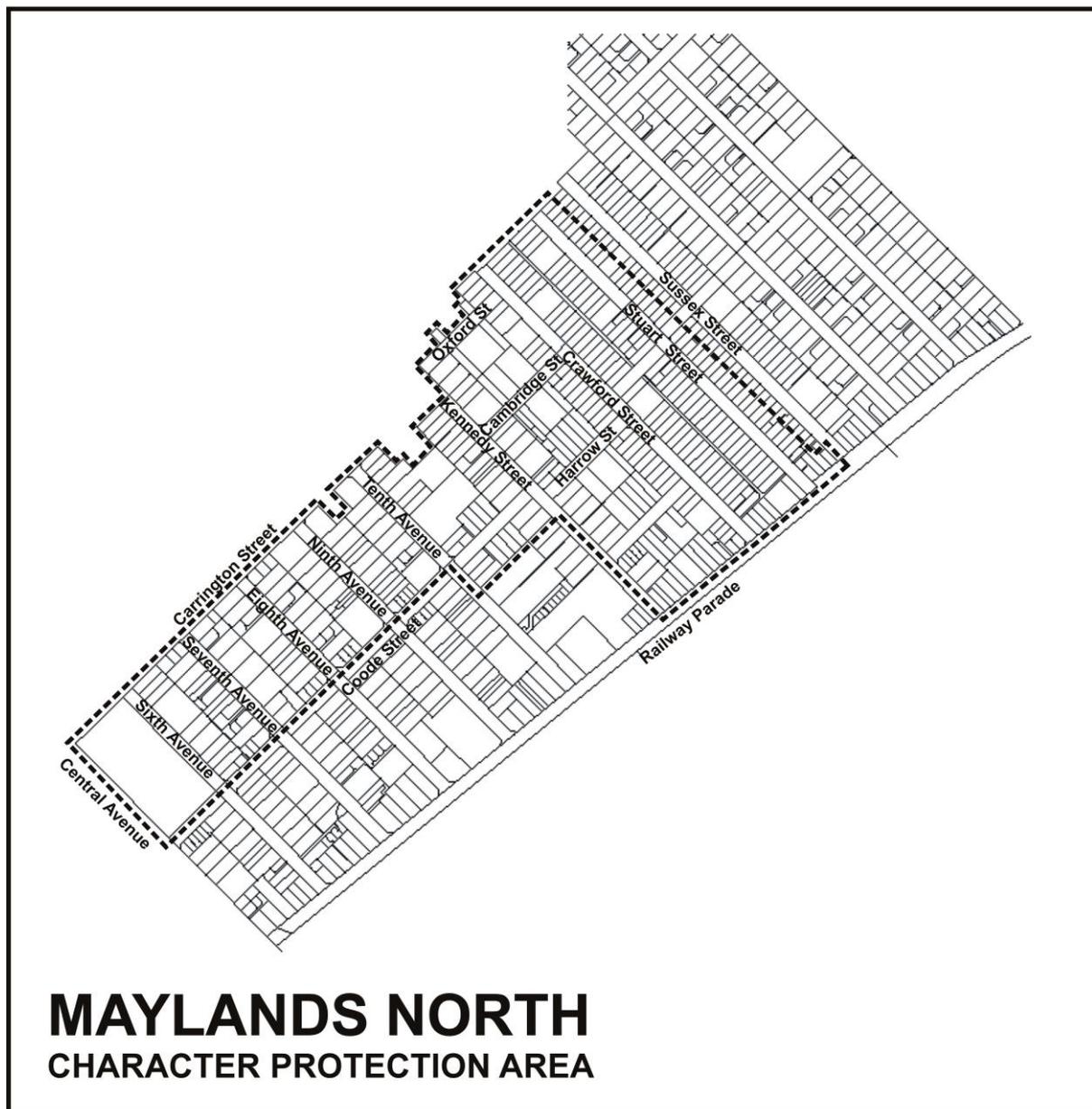
Residential Design Codes of Western Australia

Relevant City of Bayswater Town Planning Scheme No 24 provisions and associated Policies

2.0 CHARACTER PROTECTION AREAS

This policy shall apply within the areas designated in Figures 1, 2 and 3.

FIGURE 1



2.1 Character Protection Area 1: Maylands North

Character Statement

Maylands Character Protection Area is located in the south west section of the City of Bayswater. The CPA is broadly bounded by current adjacent local government boundaries to the west and south, Coode Street and Railway Parade to the east and Sussex Street to the north.

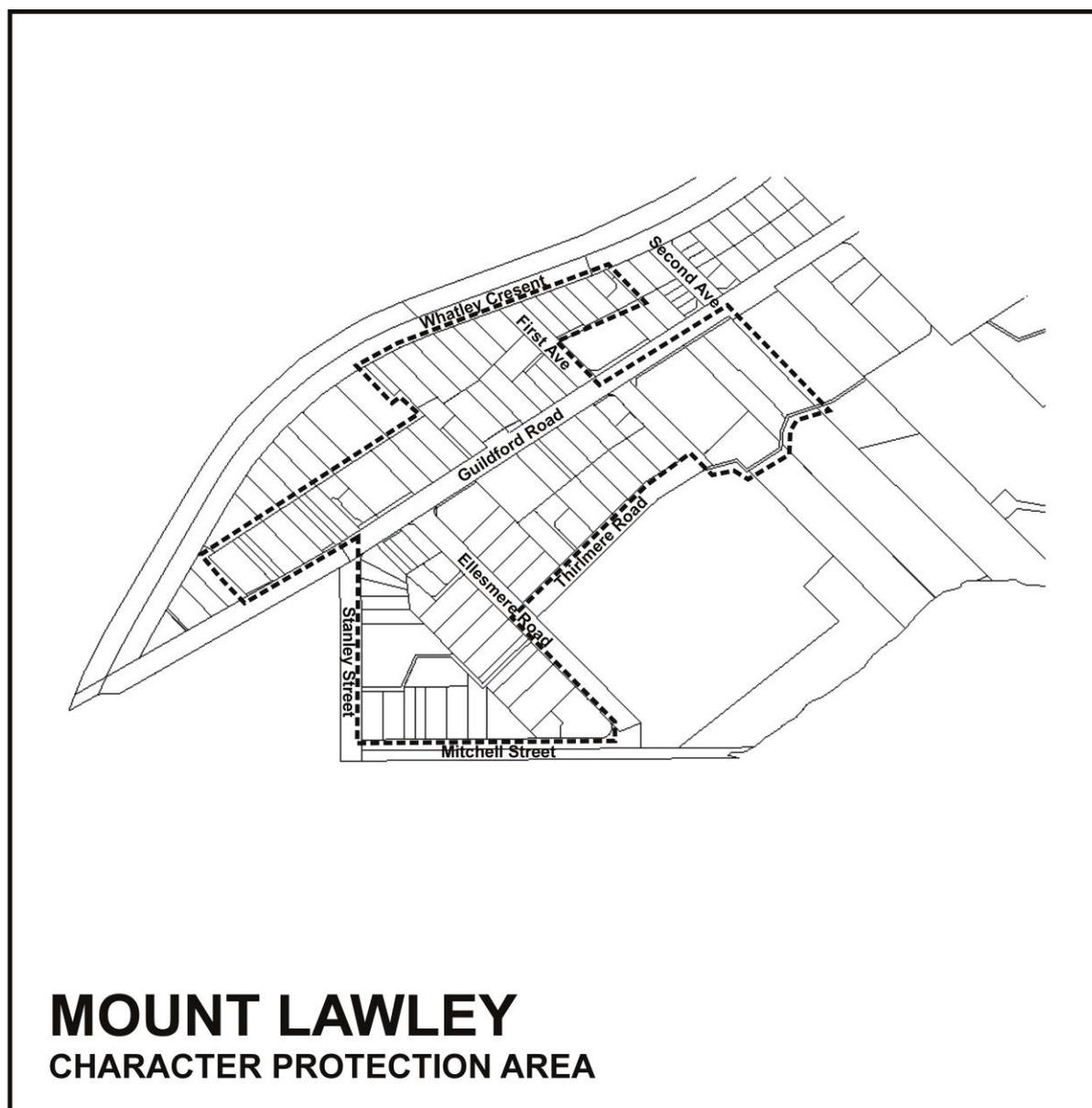
The area retains a high percentage of places that positively contribute to the streetscape. The area contains some good examples of brick and timber bungalows including the architectural styles of Federation Bungalow, Federation Timber Bungalow and Federation Timber Workers Cottage.

The landform rises from a low in the east (Railway Parade) to a high in the west (Carrington and York Street).

The predominant building form, materials and streetscape characteristics in the area are:

- Single storey with some two storey
- Asymmetrical facades and single fronted facades
- Hip and gable roof forms and tiled and corrugated steel roof cladding
- Face brick, rendered brick and timber walls. Some masonry dwellings with limestone foundations
- Front entry porches and verandahs, some with return verandahs
- Timber detailing to verandahs
- Face brick, rendered brick, brick pier and iron infill panels to fences. There are some timber and some stone fences
- Grass to front yards with perimeter planting
- Grass verge and mature street trees
- Concrete and brick paved driveways to one side of the lot

FIGURE 2



2.2 Character Protection Area 2: Mount Lawley

Character Statement

Mount Lawley Character Protection Area is located in the south west corner of the City of Bayswater. The CPA is broadly bounded by current adjacent local government boundaries to the west and south, the railway to the north and the suburb of Maylands to the east.

The seven streets in the CPA retain a high percentage of places that positively contribute to the streetscape. The area contains some good examples of brick bungalows including the architectural styles of Federation Bungalow and Federation Queen Anne Bungalow. There are a number of places with strong inter war influences including the Inter War Domestic Revival architectural style.

Guildford Road and Whatley Crescent are both high traffic volume roads that influence the character of the area.

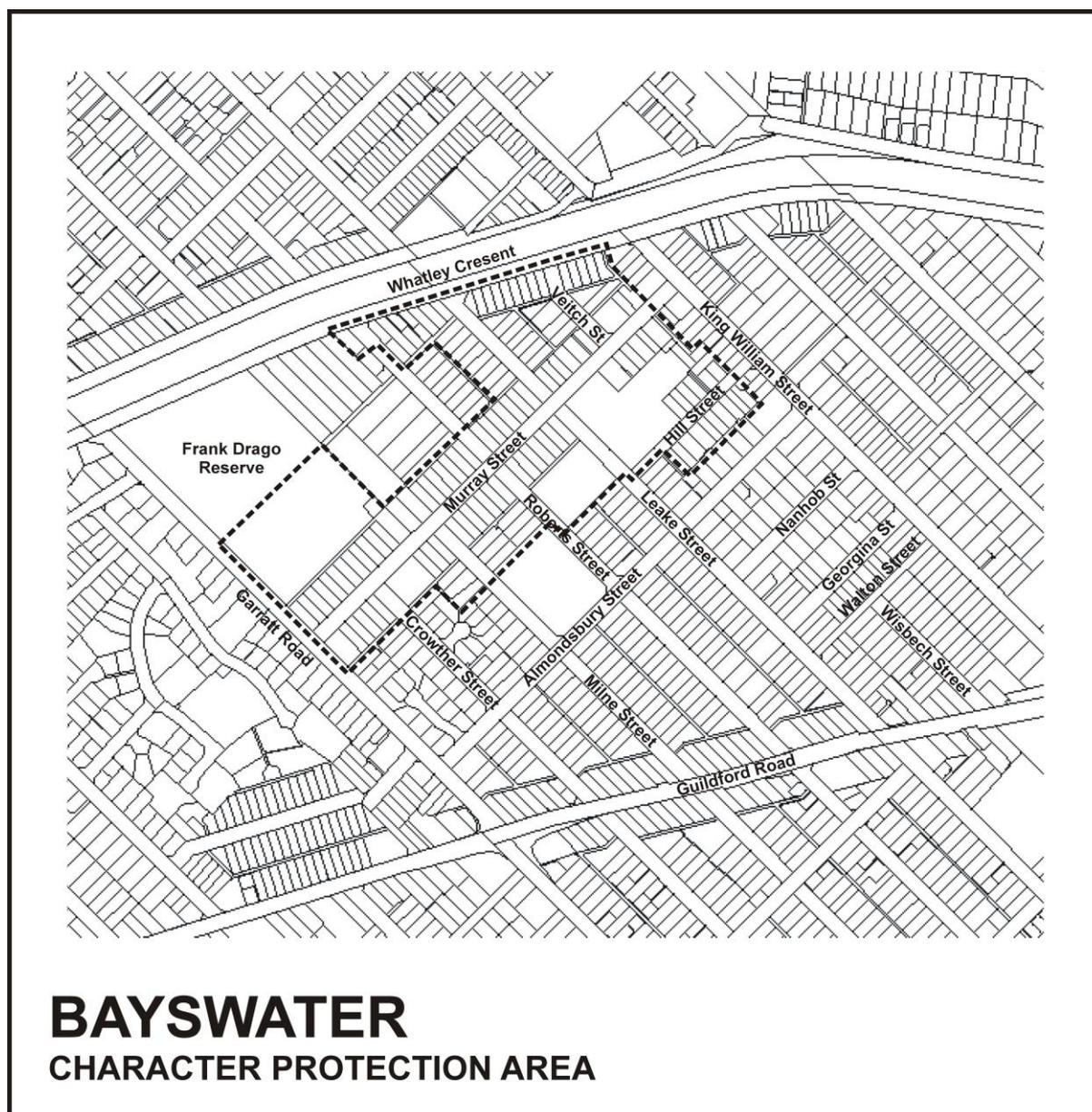
The predominant building form, materials and streetscape characteristics in the area are:

- Single storey with some two storey
- Hip and gable tiled roof forms. There are some corrugated steel roofs, especially north of Guildford Road
- Face brick or rendered brick walls, some with limestone foundations
- Front entry porches and verandahs, some with return verandahs
- Face brick, rendered brick, brick pier and iron infill panels to fences. There are some timber and some stone fences. Some higher brick and rendered brick fences to Guildford Road
- Grass to front yards with perimeter planting
- Grass verge and mature street trees
- Concrete and brick paved driveways to one side of the lot

Guildford Road and Whatley Crescent are both high traffic volume roads that influence the character of the area. These streetscapes are predominantly open with single storey dwellings dating from the late nineteenth and early twentieth century through to more recent development. The verge and a number of front setbacks of Guildford Road are concrete or brick paved. The landmark building, Albany Bell Castle, is set well back from the Guildford Road boundary.

Whatley Crescent retains grass verges, front boundary fences and perimeter planting to the front yards.

FIGURE 3



2.3 Character Protection Area 3: Bayswater

Character Statement

Bayswater Character Protection Area is located in the south west section of the City of Bayswater. The CPA is broadly bounded by Garratt Road to the south, Whatley Crescent to the west, the rear boundary of commercial properties to King William Street to the north and a line to the east from the rear of properties to Hill Street extending to Garratt Road.

The area retains a high percentage of places that positively contribute to the streetscape. The area contains some good examples of brick and timber bungalows including the architectural styles of Federation Bungalow, Federation Timber Bungalow, Federation Timber Workers Cottage and Inter War California Bungalow.

The landform rises from a low point in the north (King William Street) to a high at approximately Roberts Street.

The predominant building form, materials and streetscape characteristics in the area are:

- Single storey with some two storey
- Asymmetrical facades and single fronted facades
- Hip and gable roof forms and tiled and corrugated steel roof cladding
- Face brick, rendered brick and timber walls. Some masonry dwellings with limestone foundations
- Front façade verandahs, some with return verandahs. Some entry porches
- Awnings to some windows
- Face brick, rendered brick, timber, stone, brick pier with iron or timber infill panels to fences. There are some timber post and rail with wire infill fences and some dwellings with no fence to the front boundary. Some high brick fences to Whatley Crescent
- Grass to front yards with perimeter planting
- Grass verge and mature street trees
- Concrete and brick paved driveways to one side of the lot
- Numerous dwellings have access to rear laneways

3.0 CHARACTER

Objective:

- *To ensure that new development is in harmony with the existing character of residential development within each of the three Character Protection Areas.*

3.1 Context

New residential development should respond to the scale and mass of surrounding development and should be articulated to ensure unsympathetic contrasts of scale and mass are avoided when viewed from the street.

3.2 Subdivision Pattern

In areas where the traditional subdivision pattern contributes to the character of the streetscape new development should respect and respond to this pattern. The existing arrangement of lots, particularly the length of street frontages, generally establishes a consistent rhythm to the streetscape. In order to ensure new development respects this pattern amalgamation will not be supported in CPAs.

3.3 Building Orientation

Building orientation shall generally be consistent with the existing street pattern (that is at right angles to the street). Buildings which are angled to the street undermine the continuity of the streetscape, sit uncomfortably with their neighbours and introduce atypical front gardens.

3.4 Bulk, Form and Scale

New development should be articulated to break down their perceived bulk and establish a scale appropriate to existing dwellings in the locality when directly visible from the street. Unsympathetic contrasts of scale with adjoining dwellings shall be avoided.

3.5 Roof Pitch and Form

Roofs shall generally be pitched and articulated to ensure that the scale of individual roof elements is comparable with the scale of existing roofs in the locality.

3.6 Topography

New dwellings shall respond sympathetically to the topography and cutting and filling, particularly in locations directly visible from the street, shall be kept to a reasonable minimum.

4.0 STREETScape

Objective:

- *To retain the open character of streetscapes and landscaped gardens.*

4.1 Open Streetscapes

Dwellings should interact with the public domain and blank walls, heavy planting, screen walls, or garages and carports in the front setback area are generally to be avoided. The property boundary shall be clearly demarcated by a fence or planting.

4.2 Fences

4.2.1 Fences Generally

Fences on the front property boundary and along the side boundaries within front setback areas shall generally be low or of open construction.

4.2.2 Open Fences

Open fences are permitted on the front boundary to a height of 1m above the adjoining pavement level.

4.2.3 Masonry Fences

Masonry fences above 600mm from the adjoining pavement level shall be visually permeable (minimum 75% open). Masonry piers shall be limited to a maximum height of 1800mm above the adjoining pavement level and limited to a maximum size of 350mm x 350mm.

4.2.4 Fences on Sloping Sites

Masonry fences on sloping site may be up to 750mm above the adjoining pavement level although masonry piers shall be limited to a maximum height of 1800mm above the adjoining pavement level.

4.2.5 Materials

Fibre cement and metal sheeting shall not be acceptable materials for front fences.

4.2.6 Meter Boxes

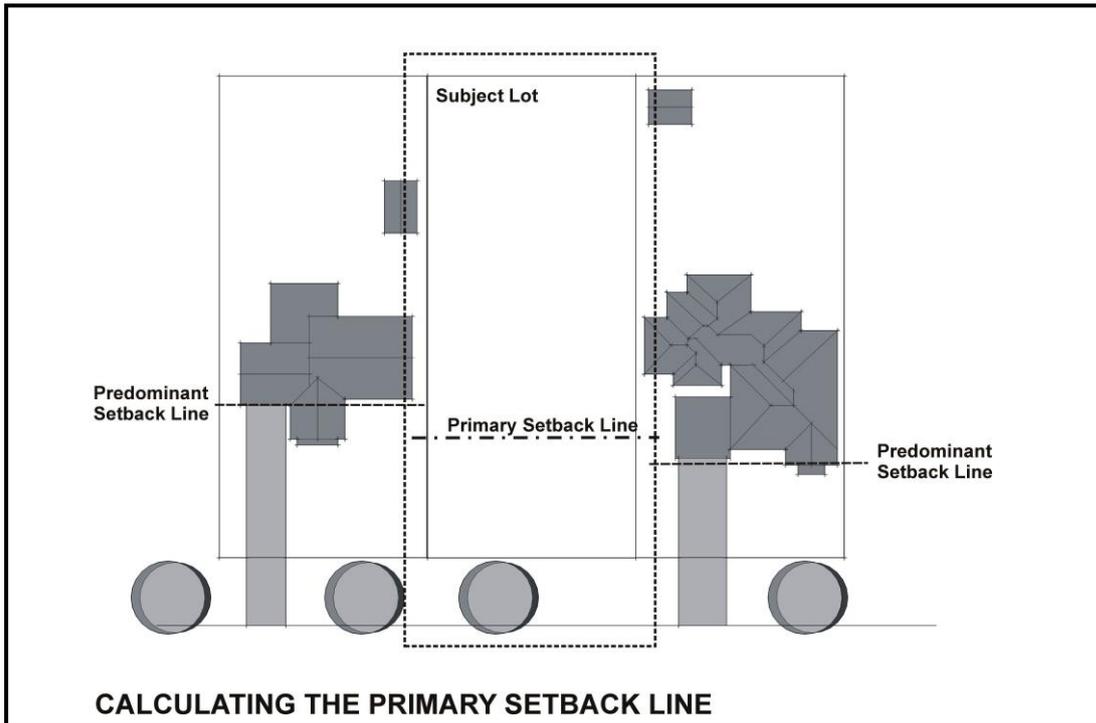
Where meter boxes are incorporated into front fences they are to be located perpendicular to the street to minimise their visual impact from the street.

4.3 Street Setbacks

4.3.1 Primary Street Setback

The primary street setback is to reflect the predominant streetscape pattern in the immediate locality. This pattern is to be taken as the average setback of the two dwellings on each side of the property the subject of the application. This average shall be measured to the predominant street setback lines of the adjacent dwellings (ie not the line of the verandahs or carports)(See Figure 4).

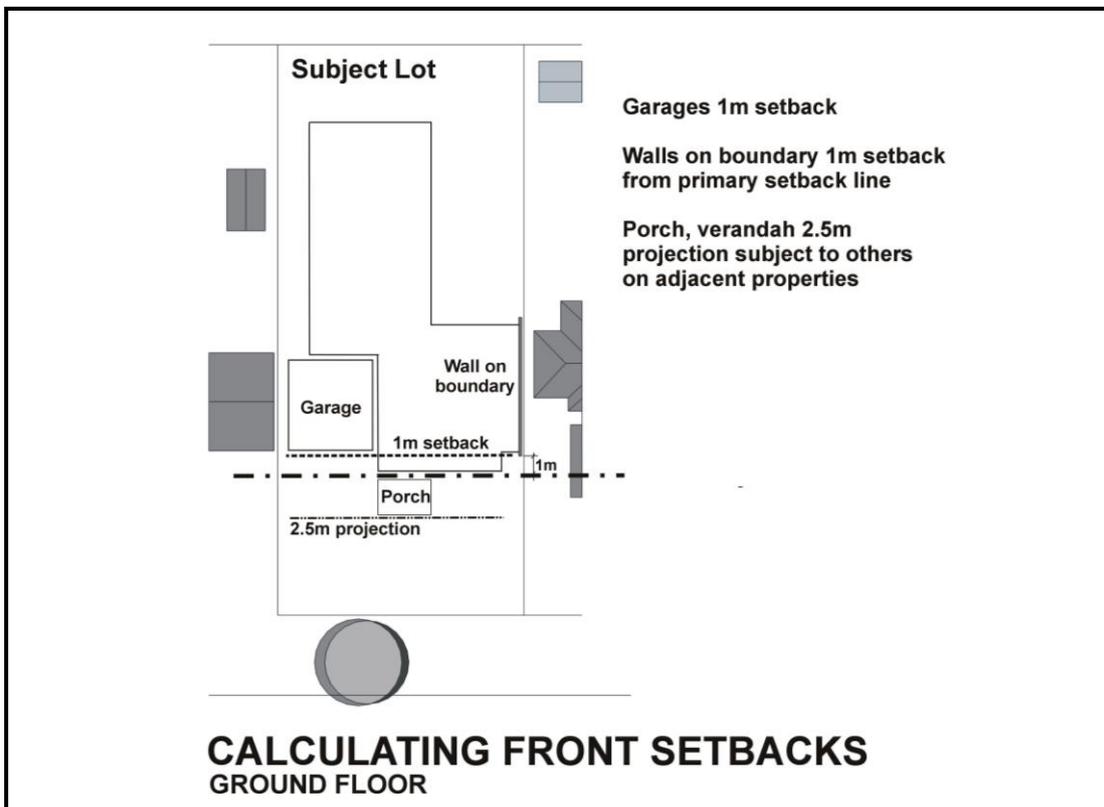
FIGURE 4



4.3.2 Open Entry Porches and Verandahs

Open entry porches and verandahs directly visible from the street shall be limited to single storey structures and shall be restricted to a maximum height of 3.5m.

FIGURE 5



Such structures can protrude into the primary front setback by 2.5m if that protrusion is reflective of existing verandahs in the immediate locality (ie occurs in at least two dwellings of the two either side of the subject site and the three across the street) and will not dominate the appearance of the dwelling (See Figure 5).

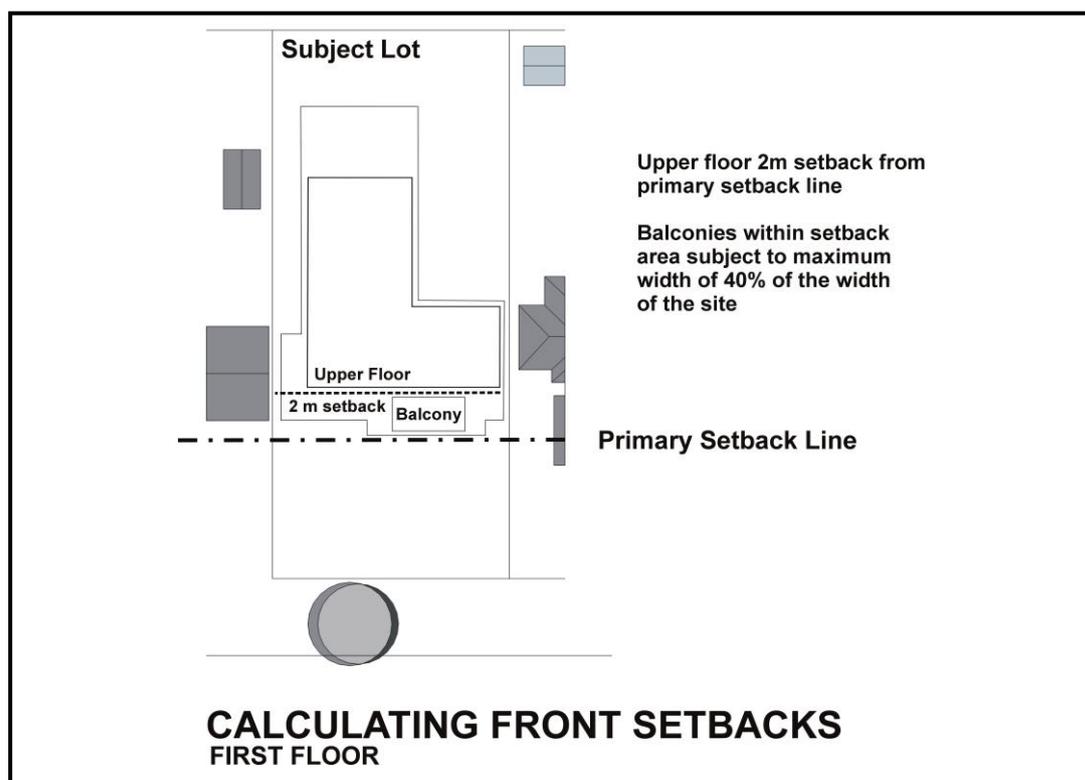
4.3.3 Garages

Garages shall be setback a minimum of 1m from the primary street setback line of the proposed dwelling (See Figure 5).

4.3.4 Upper Floors

Upper floors shall be setback a minimum of 2.0m from the primary street setback line of the proposed dwelling (See Figure 6).

FIGURE 6



4.3.5 Balconies

Open upper floor balconies will be permitted within the 2.0m setback line of upper floors but are not to dominate the street elevation of the proposed dwelling and shall generally be limited to 40% of the width of the subject site (See Figure 6).

4.4 Location of Entries

Front doors shall generally face the street. Entry doors located at the side of the dwelling may be considered but only where the entry arrangements are clear and accessed via a porch or verandah easily visible from the street.

4.5 Landscaping

4.5.1 Existing Mature Trees

Existing mature trees on the site are to be retained wherever possible. If mature trees are removed the planting of replacement trees is encouraged.

4.5.2 Landscaping of Front Gardens

Landscape plans are to include soft landscaping in the front setback areas to maintain the landscape character of front gardens in the locality.

5.0 CARPORT AND GARAGES

Objective

- *Carports and garages are not to detract from the residential character of streetscapes and shall be designed to minimise their impact on the dwellings which they serve.*

5.1 Access from Right of Ways

Garages and carports are to be located at the rear of the property and accessed via a right of way where a right of way is present and rights of access exist.

5.2 Street Trees

Street trees shall be retained. If this is not feasible they shall be replaced to the satisfaction of the City of Bayswater.

5.3 Crossovers

Single garages and carports are to be accessed from a single crossover to the street with a maximum width of 3.0m.

Double garages and carports are to be accessed from a single crossover to the street of a width of 4.8m.

5.4 Garages

Where access off a right of way is not possible garages:

- Should be integrated into the development and setback a minimum of 1m behind the line of the primary setback line of the dwelling (excluding verandahs and entry porches).
- The form and materials of garages are to be chosen to minimise their impact on the streetscape.

If a garage is provided no carport shall be permitted in the front setback area.

5.5 Carports

Where access off a right of way is not possible carports:

- May be located within the street setback area provided they are open on all sides (except where they adjoin the dwelling) and of simple and unobtrusive design to ensure they do not obscure the view of the existing dwelling from the street.
- Carport structures shall generally be independent of the existing dwelling.
- Where the carport structure adjoins the dwelling they are to be designed in a manner that does not unduly impact on the form or streetscape presentation of the existing dwelling.
- Where the primary street frontage of a lot is 12m or less only single width carports shall be permitted.
- Roofs to carports shall generally be pitched (with double pitches to double garages and double or single pitches to single carports). Alternative forms for carport roofs may be considered where they are sympathetic and complement the form of the existing dwelling. No infill panel or closing in of gable ends shall be permitted and roof pitches are to match that of the existing dwelling. While the underside of roofs may be lined no flat ceilings will be approved.
- Structural posts and columns supporting carports shall be a minimum of 150mm wide and a maximum of 300mm wide.
- Roller doors, tilt doors, and solid gates will not be permitted for any carports located within the street setback area.
- While discretion is provided in terms of material choice they shall be chosen to minimise both the impacts on the streetscape and the degree to which they obscure the existing dwelling.

6.0 BUILT FORM

Objectives:

- *To ensure that the built form of new development is sympathetic to the scale and form of adjoining buildings in the relevant Character Protection Area.*

6.1 Building Form and Scale

Strong emphasis will be placed on promoting high quality design which is sensitive to the scale and character of its context.

Building height and bulk are to be distributed appropriately on the development site to protect the amenity of adjoining lots and maintain the visual integrity of the streetscape.

The visual bulk of two storey buildings should be minimised through articulation of larger wall lengths, and the stepping back of upper storey walls.

6.2 Roofs

Hipped or gabled roof forms are preferred roof forms for any portions of buildings which contribute to the immediate streetscape, unless it can be demonstrated that the roof form proposed will not detract from the established streetscape.

6.3 Retaining walls

Retaining walls within the front setback area are to be stepped vertically if they are more than 0.5 m high to reduce their visual impact.

6.4 Boundary Walls

Boundary walls are to be setback a minimum of 1m behind the primary street setback line or the front wall of the dwelling where this is setback beyond the primary street setback line.

6.5 Materials

New dwellings should recognise the characteristic materials, textures and colours used in the character protection areas. These should be re-interpreted and incorporated as part of the new development.

Materials and colours of surrounding buildings should not be simply copied but used as a point of reference. Modern materials can be used if their proportions and details are harmonious within the surrounding character. Colour, texture and tonal contrast can be unifying elements. The outcome of the total composition should make a positive contribution to the existing streetscape.

6.6 Detailing

Detailing should comprise the direct and simple expression of architectural elements with a limited palette of materials used to provide visual interest and articulate the building form.

Roofs should generally comprise gabled or hipped forms disaggregated into elements which will break up the scale of roof elements. They should incorporate overhanging eaves and include components such as cover over entrances and canopies over windows which will assist in articulating the built form.

The relationships of solids to voids should reflect that of the immediate locality and detailing should be used to provide identity and highlight important components such as entrances, chimneys, balconies, and to screen service areas.

Fencing and landscaping should comprise integral elements of the overall design.

6.7 Air Conditioning Equipment and Plant, Satellite Dishes and Solar Panels

Air conditioning equipment and plant and satellite dishes are to be located in positions where they are not directly visible from the street.

Solar panels are permitted. However where practicable they shall be located in positions which minimise any undue impact on the streetscape.

7.0 ALTERATIONS AND ADDITIONS

Objectives:

- *To ensure that alterations and additions are sympathetic to existing dwellings.*

Provisions

7.1 Additions to Complement Existing Dwelling

Additions to existing dwellings should be clearly distinguishable and complement the existing dwelling (See Figure 7).

7.2 Front Setbacks

No additions and/or alterations to existing dwellings shall occur within the primary front setback area.

7.3 Ground Floor Additions

Any ground floor additions to an existing dwelling shall be setback a minimum of 2m from the primary setback line (verandahs not included).

The use of links, courtyards and breezeways are encouraged. These minimise the impacts on the existing dwelling and provide improved internal and external living conditions (See Figure 7).

Alternatively additions which are contiguous with the existing dwelling should seek to minimise the impact on the existing dwelling (See Figure 7).

7.4 Second Storey Additions

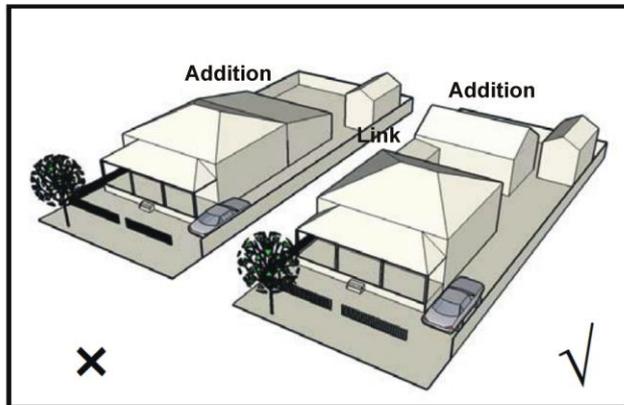
Second storey additions shall generally be located to the rear of the ridgeline of the roof of the existing dwelling (See Figure 7).

Two storey extensions at the front of the existing dwelling that dominate the streetscape will not be supported (See Figure 7)

7.5 Additions not Visible from the Street

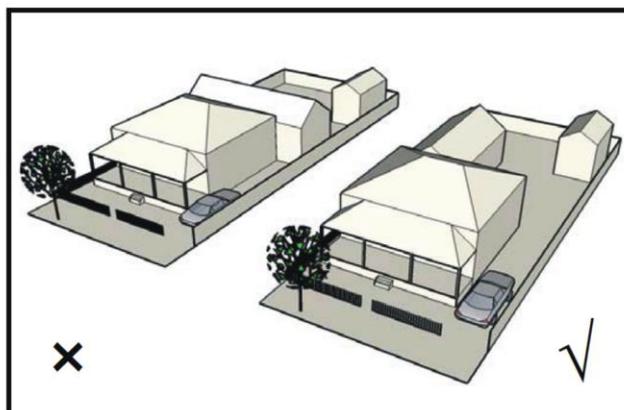
Additions not directly visible from the street may have a different character to the architectural style of the existing dwelling, including contemporary design, but shall be sympathetic to the form, scale, and bulk of the existing dwelling and development in the immediate locality.

FIGURE 7

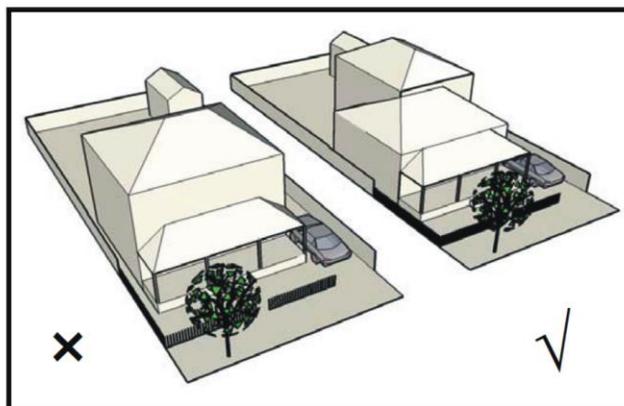


ALTERATIONS AND ADDITIONS

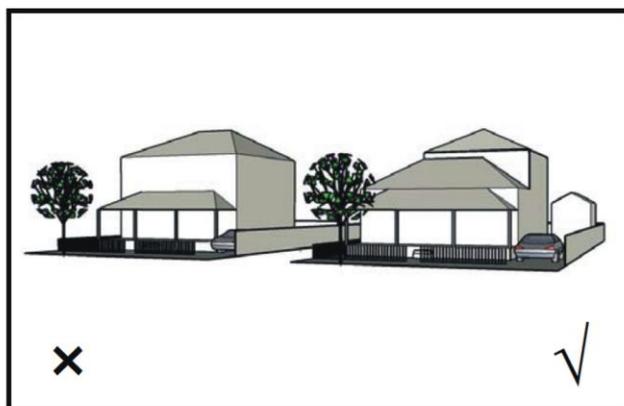
The use of links, courtyards and breezeways are encouraged. These minimise the impacts on the existing residence and provide improved internal and external living conditions



Alternatively contiguous extensions should seek to minimise the impact on the existing residence



Two storey extensions should be placed behind the ridgeline of the existing residence



Two storey extensions at the front will dominate the streetscape and will not be supported

8.0 GROUPED DWELLINGS

Objectives:

- *To ensure new group housing developments make a positive contribution to the streetscape.*

Provisions

The design of any areas of grouped dwelling developments which will be directly seen from the street are to comply with the provisions of Section 3.0, 4.0, 5.0 and 6.0 of this policy.

9.0 MULTIPLE DWELLINGS

If multiple dwellings are permitted within the CPA the following provisions shall apply.

Objectives:

- *To ensure the built form and architectural expression of multiple dwelling developments preserve the character of the relevant Character Protection Area and are sympathetic to the character of adjoining residences in the street.*

Provisions

9.1 Street Setback

The primary street setback is to reflect the predominant streetscape pattern in the immediate locality. This pattern is to be taken as the average setback of the two dwellings on each side of the subject lot. This average shall be measured to the predominant street setback lines of the adjacent dwellings (ie not the line of the verandahs or carports)(See Figure 4).

9.2 Built Form

The built form of multiple dwellings is to be articulated into perceptually differentiated components which reflect the scale of adjoining dwellings. That is walls, openings and roof planes are to be modulated in response to the scale of neighbouring buildings. Such forms are to have a vertical emphasis.

The ground floor of the development shall have a clear and direct relationship to the existing ground or street plane and entrances shall generally face the street.

Entries located at the side of the development may be considered but only where the entry arrangements are high legible and accessed via a defined porch or verandah easily visible from the street.

Materials and details shall comply with Sections 6.5 and 6.6 of this policy.

Service elements such as satellite dishes, solar panels and water tanks shall be located so that they are not directly visible from the street or shall be adequately screened.

9.3 Roofs

The roof form and skyline shall be articulated by ridge and parapet lines, roof slopes, and punctuated by party walls, chimneys, skylights or other elements to break down the scale and the perceived height of the building. Roof forms facing the street shall be sympathetic to existing roofs in the immediate vicinity.

9.4 Landscaping

Landscaping in the front setback area shall include soft landscaping to maintain the landscape character of front gardens in the area. These areas shall generally retain existing ground levels within the front setback area.

New fences, garden walls, planting and landscape elements shall reflect the scale of such elements in the immediate area.

10.0 ACCESS AND CAR PARKING

Objectives:

- *Vehicular access and car parking should be designed to minimise the impact on streetscapes.*

Provisions

Garages and carports are generally to be located at the rear of the property and accessed via a right of way where a right of way is present and rights of access exist.

Where access to garages or carports is taken from the primary street the provisions set out in Clause 5.0 above shall apply.

In grouped or multiple dwelling developments:

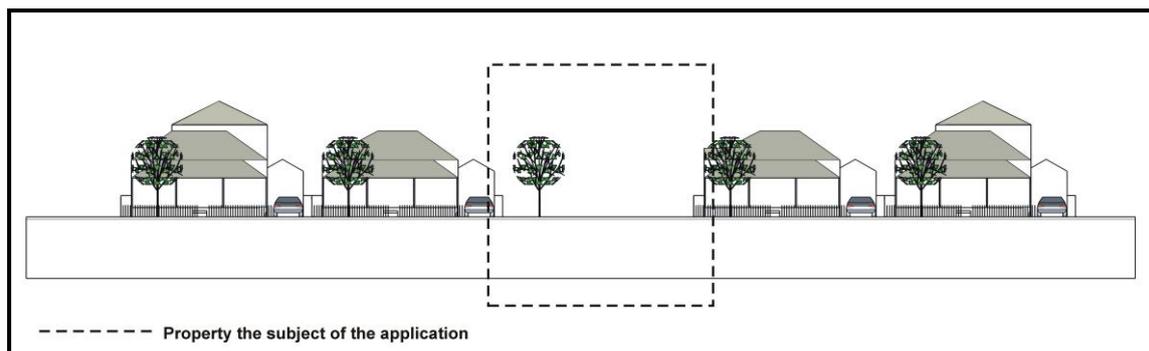
- The alignment of access ways should be varied to avoid the 'gun barrel' effect.
- Parking areas shall be located within developments and not in the front setback area.
- Parking areas shall be screened from the street by buildings or landscaping.
- Parking provision for more than six bays shall be broken up with trees, buildings, or different surface treatments.

11.0 APPLICATION FOR PLANNING APPROVAL

An application for planning approval for development of a lot within the Character Protection Areas is to be accompanied by the following information in addition to that required by the Town Planning Scheme:

1. Colour photographs of the elevations of the existing dwelling and development on the two adjoining properties on either side of the subject property.
2. A streetscape elevation of the proposed development which includes development on the two adjoining properties on either side of the subject property at a scale not less than 1:100 (See Figure 8).

FIGURE 8: STREETSCAPE ELEVATION



3. A schedule of finishes of all materials which could be seen directly from the street.
4. A landscape plan of the front garden area proposed indicating proposed fencing and identifying both materials (paving) and plant species.

12.0 ADVERTISING OF APPLICATIONS

Where an application is made for planning approval to carry out development which in the opinion of the City of Bayswater does not comply with the relevant provisions of the Character Protection Area policy the City may give notice of the application in accordance with the provisions of sub-clause 3.3.2 of the Residential Design Codes.

DEFINITIONS

Bulk

The combined effect of the arrangement, volume, size and shape of a building or group of buildings.

Character

Character is defined by the combination of the particular characteristics or qualities of a place. These include the rhythm, scale and visual amenity of dwellings which contribute to the existing streetscapes.

Context

The specific character, quality, and physical characteristics of a building's setting.

Form

The form of a building is its overall shape and volume and the arrangement of its parts.

Mass

The size and volume of a building.

Predominant setback

The predominant setback of an existing residence comprises the setback from the street boundary of the major portion of the front façade. It excludes projecting verandahs, bay windows, garages and carports.

Primary street setback

The primary street setback is the predominant streetscape pattern in the immediate locality. This is to be taken as the average setback of the two dwellings on each side of the property the subject of the application. This average shall be measured to the predominant street setback lines of the adjacent dwellings (ie not the line of the verandahs or carports)

Scale

The size of a building and its proportional relationship with its surrounding dwellings in the locality.

Streetscape

All of the visual elements which inform the perception of a street; these include the road, front fences and gardens, existing buildings scale and form, roofscapes, trees and open spaces, that combine to form the street's character.

Visual Amenity

All those visual factors which assist in forming the character of an area and contributing to its current and likely future amenity.